



AREA STRUCTURE PLAN



Prepared for: City of Lloydminster
Presented by: Select Engineering Consultants Ltd.
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1.0 Introduction

1.1 Vision

Hill Industrial provides new business and employment opportunities in the City of Lloydminster. The designation of this area for industrial land use will ensure a supply of serviced and accessible industrial and commercial lots in the future.

1.2 Purpose

This Area Structure Plan (ASP) provides a statutory planning framework to guide the review and approval of development applications. It conceptually identifies land uses, the location of transportation routes, alignments for public utilities and servicing and the anticipated sequence of development. The ASP also establishes development objectives, consistent with relevant City policy direction, to be implemented through development.

The interface between development within Hill Industrial and existing and proposed developments for the surrounding areas are addressed by the Plan.

This ASP provides the basis for the submission of more detailed Outline Plans, which are required by the City of Lloydminster prior to zoning, subdivision and development approvals.

1.3 Location

Hill Industrial is located in the northwest corner of the City of Lloydminster, in Alberta (See Figure 1). The ASP area consists of approximately 523 ha (1,292 ac) of land and is bounded by:

- 75 Avenue (which also forms the City Boundary) to the west;
- 59 Avenue to the east;
- 44 Street (Highway 16) to the south; and
- 67 Street (which also forms the City Boundary) to the north.

1.4 Land Ownership

The eastern developed portion of Hill Industrial is held by numerous landowners under multiple Certificates of Title. Lands in the western portion of the area, which are currently undeveloped, are held by three landowners under multiple Certificates of Title.

In the undeveloped portion of Hill Industrial a number of Rights-of-Way exist:

- a utility Right-of-Way (832 1735) and gas pipeline Right-of-Way (842 1210) parallel 67 Street;
- two gas pipeline Rights-of-Way (812 0401 and 842 1210) parallel 75 Avenue;
- two gas pipeline Rights-of-Way (792 3133 and 5101 NY) run east-west north of the alignment of 56 Street;
- an oil pipeline Right-of-Way (8721911) runs east-west north of the alignment of 56 Street and then runs southward paralleling 75 Avenue;
- a gas pipeline Right-of-Way (3111 KS) runs east-west north of the alignment of 52 Street; and
- a power line Right-of-Way (1926 KS) runs north-south paralleling the alignment of 67 Avenue.

1.5 Stakeholder Consultation

As part of the preparation of the Hill Industrial ASP, a Stakeholder Consultation and Public Information process has been implemented.

A public meeting was held on October 12, 2013, to inform the public of the preparation of the ASP to provide opportunity for community involvement and feedback.

Notification to the public meeting was advertised in the "Meridian Booster" and "The Source" newspapers. The advertisement ran for two consecutive weeks prior to the meeting date, inviting any and all interested residents of Lloydminster to attend. Public agencies were also individually notified of the public meeting and the opportunity to provide feedback.

2.0 Site Context and Development Considerations

2.1 Existing Land Uses

The developed lands within Hill Industrial include a mix of industrial and commercial uses. Commercial development is located along 44 Street (Highway 16).

The majority of undeveloped lands within the area are currently used for agricultural purposes. Industrial development is proceeding in the northwest portion of Hill Industrial.

Three abandoned gas wells are located within the northwest portion of the area, two north of 62 Street and one south of 62 Street. All of these are former sweet gas wells which are being reclaimed. As per Energy Resources Conservation Board (ERCB) Directive 079, a minimum 5m radius setback will be required. If an additional setback is required, it will be negotiated on a case-by-case basis.

2.2 Adjacent Land Uses

2.2.1 Husky Industrial

The lands immediately east of Hill industrial, north of the CN Rail line, are planned for industrial uses. The southern portion has been developed as the Husky Energy Lloydminster Refinery.

2.2.2 West Lloydminster

The West Lloydminster neighbourhood is located south of the CN Rail line and east of 59 Avenue, to the east of Hill industrial. West Lloydminster has been developed to include residential, institutional, commercial and industrial land uses.

2.2.3 Southridge

The Southridge neighbourhood is located south of 44 Street (Highway 16) and east of 62 Avenue, to the southeast of Hill industrial. Southridge has been developed to include residential, institutional, commercial and industrial land uses.

2.2.4 Parkview Estates

The Parkview Estates neighbourhood is located south of 44 Street (Highway 16) and west of 62 Avenue / 59 Avenue, to the south of Hill industrial. Parkview Estates is being developed to include residential, institutional and commercial land uses.

2.2.5 West Commercial

The West Commercial neighbourhood is located west of 75 Avenue, to the southwest of Hill industrial. West Commercial is being developed to include commercial and industrial land uses.

2.2.6 Lloydminster Municipal Airport

The Lloydminster Municipal Airport lies to the northwest of Hill industrial, west of 75 Avenue and north of 67 Street (Township Road 502). Access to the airport is provided via Range Road 12 and Township Road 502A.

2.2.7 County of Vermilion River No. 24

County of Vermilion River No. 24 lies west and north of Hill Industrial and the City Boundary. Agricultural land uses occupy the lands bordering Hill Industrial to the west.

2.3 Existing Site Features

Lands in Hill Industrial east of 67 Avenue and south of the CN Rail line, have been largely developed for industrial uses and designated for continued industrial development. Adjacent to 44 Street (Highway 16) the lands west of the 67 Avenue alignment and the parcels east of 62 Avenue are planned and under development for commercial uses.

Undeveloped lands within Hill Industrial, west of 67 Avenue and north of the CN Rail line, are cleared with some dispersed tree stands and low-lying areas and in use for agricultural purposes. North of 56 Street, these undeveloped lands slope gradually from southwest to northeast. Elevation in this portion of the area varies approximately 16 m across the site.

3.0 Development Objectives and Principles

3.1 Land Use Concept

The Land Use Concept for the undeveloped portion of Hill Industrial forms an extension of the existing industrial uses in the eastern portion of the area (See Figure 2). The expansion and clustering of existing industrial uses creates potential to establish synergies between proximate industrial users. This could result in efficiencies between industrial activities and also reduce the burden placed on the natural environment. Hill Industrial is separated from residential development to the southeast in West Lloydminster by an open space buffer adjacent to 59 Avenue, providing an adequate separation distance between industrial and non-industrial uses.

3.2 Commercial

Immediately north of 44 Street (Highway 16), lands within Hill Industrial are primarily designated for commercial land use to support the development of vehicle oriented commercial.

3.3 Industrial

Lands within the area are primarily designated for industrial land use to support the provision of an adequate supply of industrial land for the City. A variety of business and light industrial uses are envisioned for Hill Industrial. Some commercial development may be supported in the industrial area should it be proven that it is compatible with the planned industrial uses.

Further definition of future land uses and lot sizing will be provided through the preparation of Outline Plans to support future zoning, subdivision and development within the area.

3.4 Parks and Open Space

At the time of subdivision, the City will require 5% of the Gross Developable Area to be dedicated as Municipal Reserve (MR) in the form of land and/or cash-in-lieu. Only lands above the High Water Line (HWL) of stormwater management facilities within the ASP area will be allocated for MR.

While no active recreation spaces are planned within Hill Industrial, some open space will be provided by future stormwater management facilities as well as pipeline Rights-of-Way. These spaces will provide passive recreation opportunities for users of the industrial park.

3.5 Circulation

Strong local, regional, inter-provincial and national access to the area, supporting industrial and commercial uses, is provided by the 75 Avenue, 67 Street, 62 Street and 52 Street arterial roadway connections to 44 Street (Highway 16) and 50 Avenue (Highway 17).

Access locations and design requirements will depend on the intensity of industrial land uses within Hill Industrial. No individual lot access will be provided to 75 Avenue or 67 Street. Rather, future industrial lots adjacent to these roadways will be accessed via the future internal roadway network. Shared access is contemplated for future lots fronting onto 62 Street in order to minimize access points along this arterial roadway.

Local roadways within Hill Industrial are anticipated to be developed to a rural standard with a 30.0 m cross section. All roadways will be designed to comply with the City of Lloydminster Municipal Development Standards.

A Transportation Impact Assessment (TIA) may be required to accompany an Outline Plan submission to provide additional detail regarding the provision of adequate access and circulation to facilitate development within Hill Industrial. Additional roadway access from 75 Avenue may be provided to serve the western portion of the area.

4.0 Infrastructure, Servicing and Staging

4.1 Municipal Services

Development within Hill Industrial can be immediately serviced through connections to existing services within the developed portion of the area. All municipal services within the area will be designed in accordance with the City of Lloydminster Municipal Development Standards.

4.1.1 Water Distribution

An existing 400 mm water service extends into the undeveloped northwest portion of Hill Industrial (north of 56 Street) along 62 Street, which will be extended to 75 Avenue. An additional existing 250 mm water service extends into the northwest portion of the area along 66 Street, connecting to an existing 400 mm water service in the future 68 Avenue Right-of-Way. (See Figure 3)

To service the undeveloped portion of Hill Industrial water looping is proposed within future road Rights-of-Way to connect the existing water services.

4.1.2 Sanitary Sewer Collection

An existing 375 mm sanitary service extends into the undeveloped northwest portion of Hill Industrial (north of 56 Street) along 62 Street. An additional existing 450 mm sanitary service extends into the northwest portion of the area along 66 Street, connecting to an existing 250 mm sanitary service in the future 68 Avenue Right-of-Way. (See Figure 4)

To service the undeveloped portion of Hill Industrial sanitary connections are proposed within future road Rights-of-Way to connect the existing sanitary services.

4.1.3 Stormwater Management

The northwest portion of Hill Industrial will drain eastward into a stormwater management facility located southwest of the intersection of 62 Street and the power line Right-of-Way (67 Avenue alignment). This facility has been sized to accommodate drainage for the undeveloped portion of the area north of 56 Street. The undeveloped portion of Hill Industrial south of 56 Street is anticipated to drain to a stormwater management facility in the developed portion of the area, to the west of 62 Avenue north of the CN Rail line. The final sizes of these facilities will be established at the Outline Plan, subdivision and development stages based on more detailed development concepts and grading plans. (See Figure 5)

Stormwater will be directed to the planned facilities via surface drainage in ditches within the rural road Rights-of-Way. All storm drainage from Hill Industrial ultimately drains to Neale Lake via an outlet at the northeast corner of the area.

4.2 Franchise Utilities

Franchise utilities (including gas, power and telephone service) will be extended into the undeveloped portion of Hill Industrial from existing services within the developed portion. Extension of these services will be established in detail at the development stage.

4.3 Staging

Staging of development in Hill Industrial will follow the logical and economic extension of roadways and services to the area as driven by market demand.

It is anticipated that the undeveloped portion of the area, north of 56 Street, will be generally developed from north to south (See Figure 6). Three stages are anticipated, with the first stage consisting of the area immediately north and south of 66 Street and the area south of 62 Street. Stage 2 would consist of the balance of lands north of 62 Street and the southeast portion of the lands south of 62 Street. Stage 3 includes the southwest portion of the lands south of 62 Street.

South of 56 Street, the undeveloped portion Hill Industrial is anticipated to be developed in seven stages, proceeding generally from northeast to southwest.

5.0 Land Use Statistics

Table 1: Land Use Statistics

Land Use	Ha	% GDA
GROSS AREA	523.39	
Arterial Roadways	19.73	
Pipelines / Power Lines	9.99	
Rail Right-of-Way	6.79	
GROSS DEVELOPABLE AREA	486.88	100.0%
Commercial	36.74	7.5%
Industrial	364.3	74.8%
Public Utility Lot / Stormwater Management Facility ¹	43.24	8.9%
Circulation	42.60	8.8%
Municipal Reserve (as cash-in-lieu²)	24.34	5.0%

¹ Undeveloped lands south of 56 Street will drain to a stormwater management facility within the developed portion of the ASP area, west of 62 Avenue and north of the CN Rail line.

² Lands above the High Water Level within stormwater management facilities will be credited as Municipal Reserve (MR). Final amounts of cash-in-lieu required for MR will be established at the subdivision stage.

APPENDIX A

Area Structure Plan Figures

Figure 1: Location

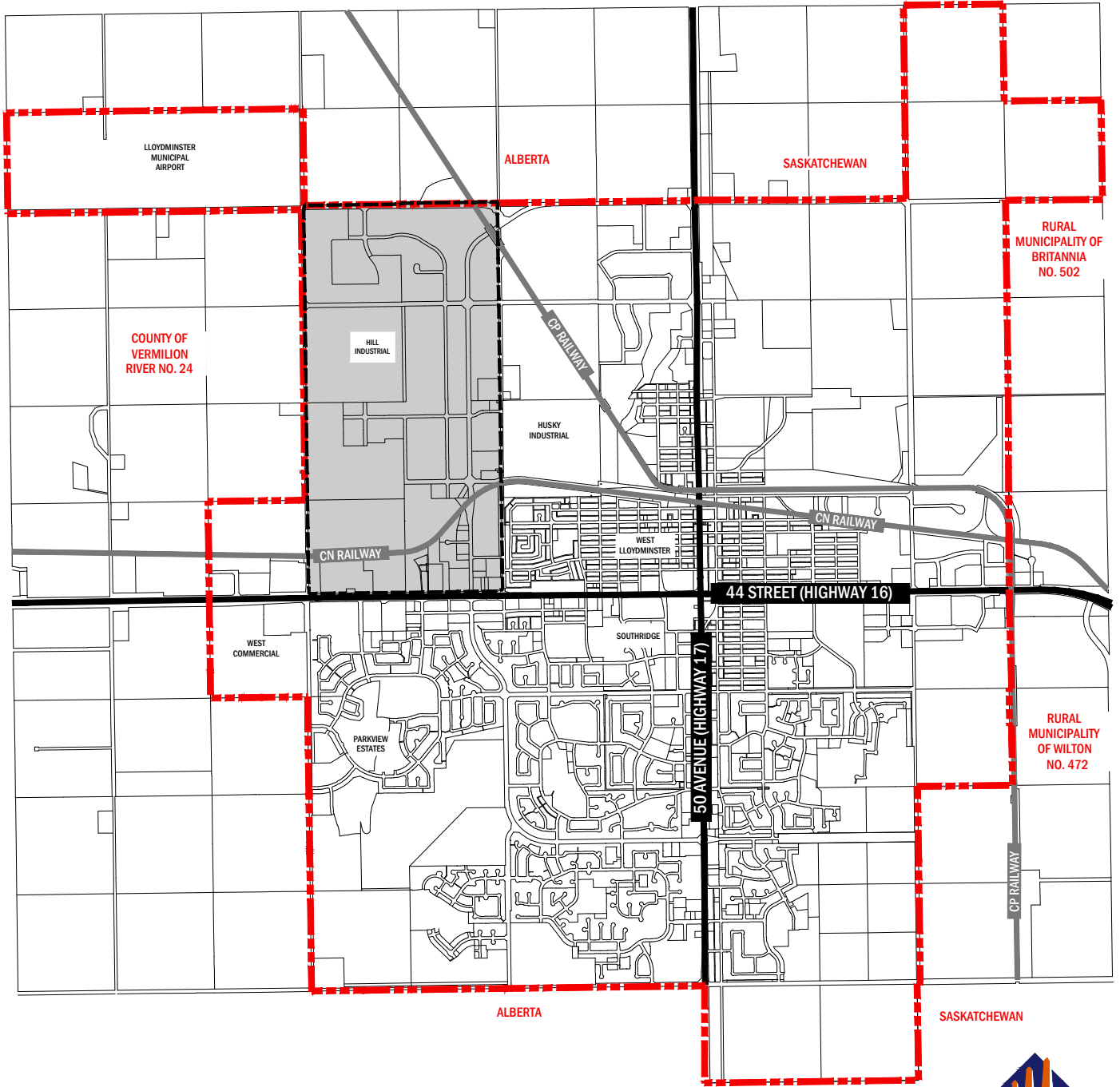
Figure 2: Land Use Concept

Figure 3: Water Servicing Concept

Figure 4: Sanitary Servicing Concept

Figure 5: Stormwater Servicing Concept

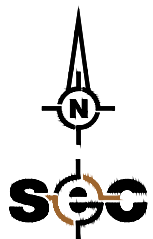
Figure 6: Staging Concept



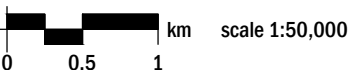
LLOYDMINSTER
Canada's Border City

LEGEND

- ASP AREA
- CITY BOUNDARY
- HIGHWAY
- RAILROAD



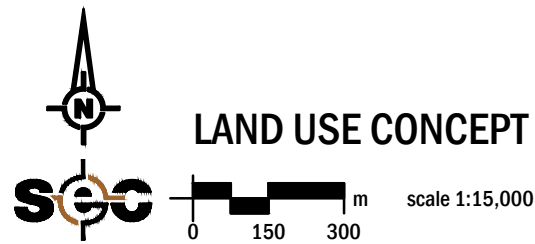
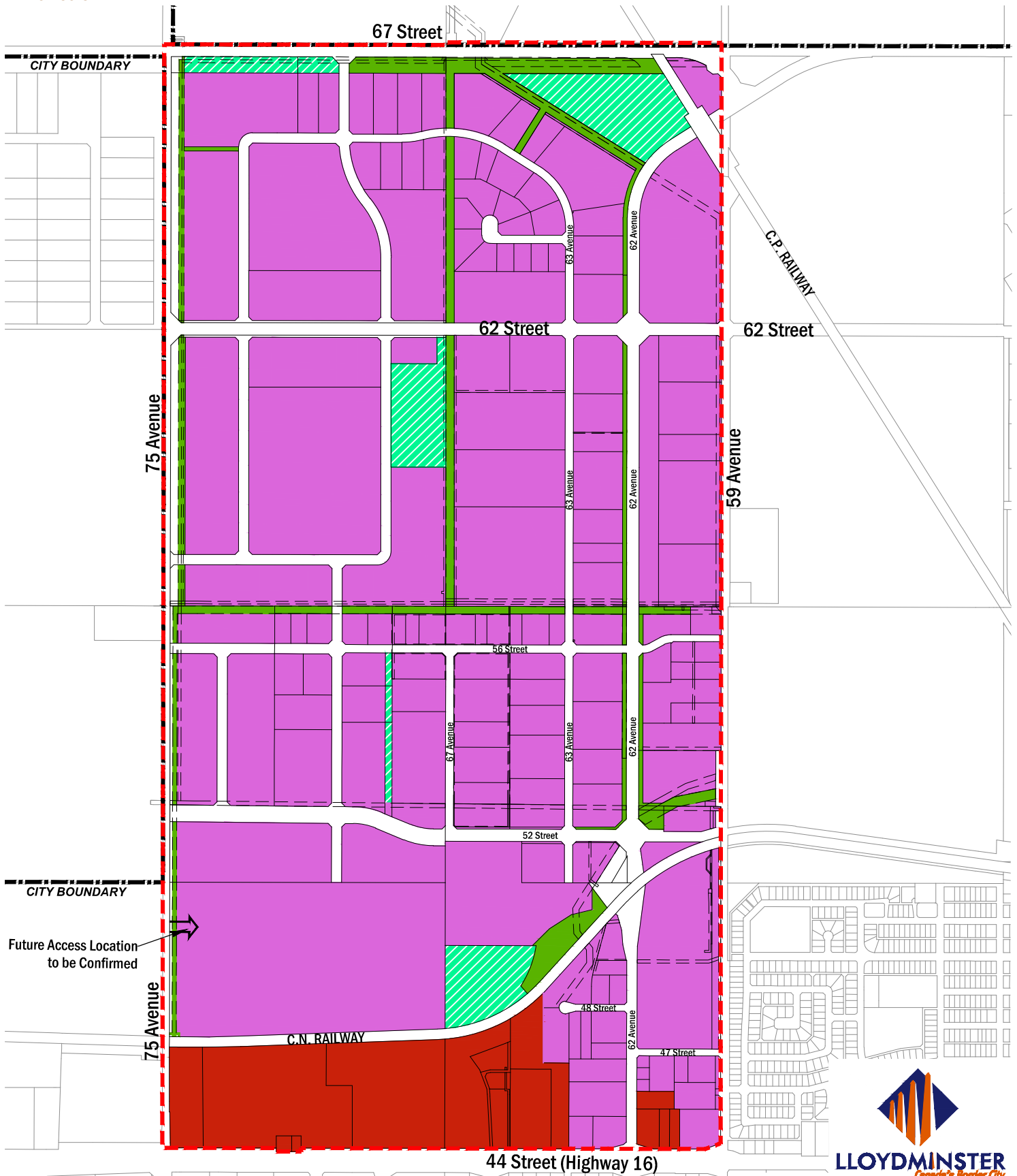
LOCATION



HILL INDUSTRIAL
CITY OF LLOYDMINSTER

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Rlashuk December 13, 2013

FIGURE 1



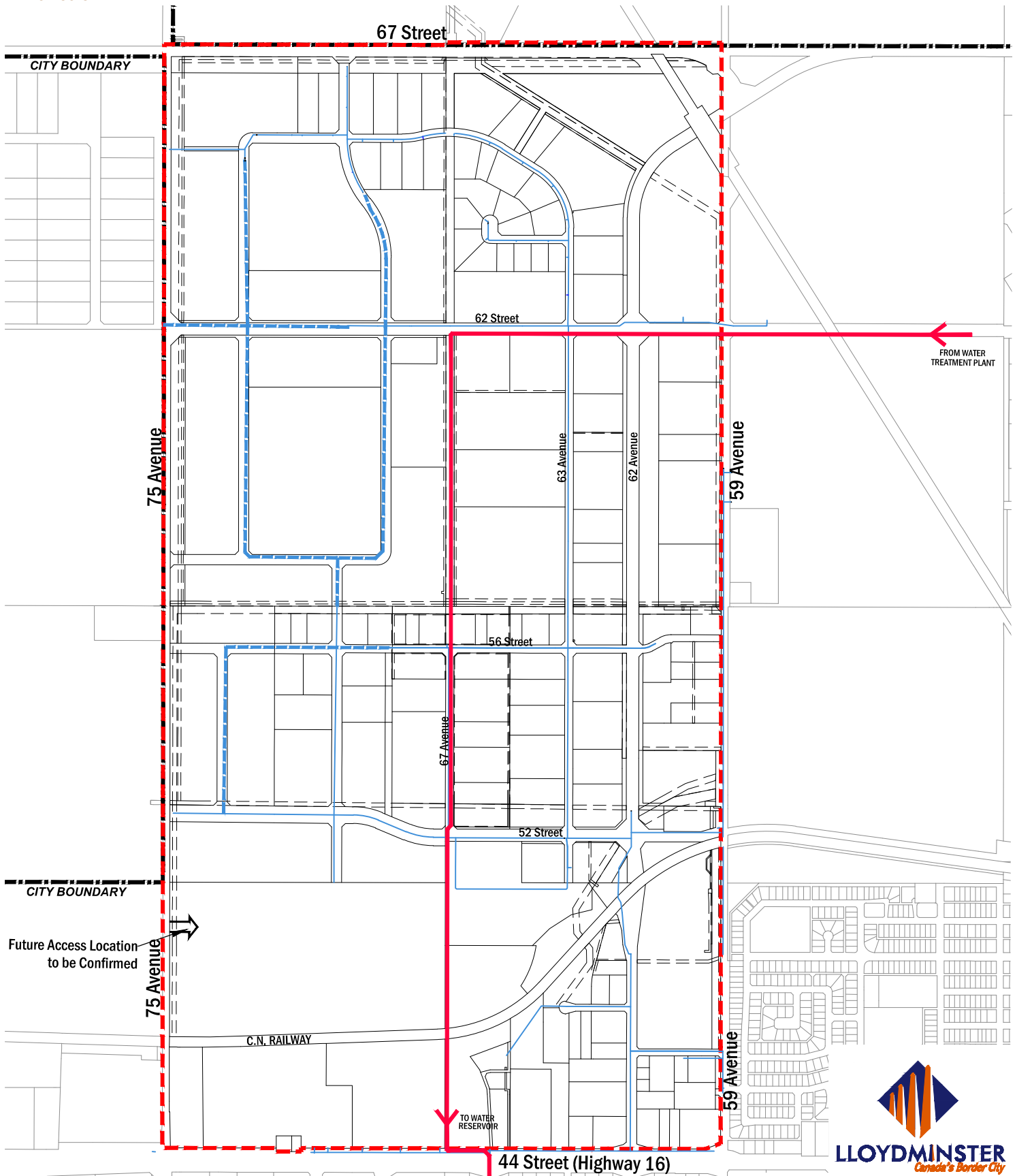
LEGEND

	ASP BOUNDARY
	COMMERCIAL
	INDUSTRIAL

	PUBLIC UTILITY LOT
	STORMWATER MANAGEMENT FACILITY

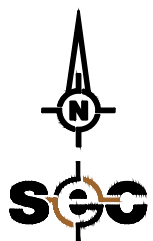
HILL INDUSTRIAL
 CITY OF LLOYDMINSTER



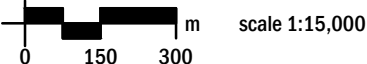


LEGEND

- - - ASP BOUNDARY
- - - PROPOSED WATER MAIN
- EXISTING WATER MAIN
- FUTURE WATER TRANSMISSION MAIN



WATER SERVICING CONCEPT



HILL INDUSTRIAL
CITY OF LLOYDMINSTER



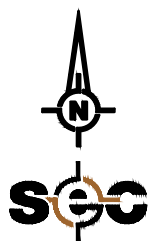
LLOYDMINSTER
Canada's Border City



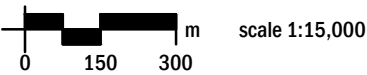
LLOYDMINSTER
Canada's Border City

LEGEND

- ASP BOUNDARY
- PROPOSED SANITARY
- EXISTING SANITARY
- PROPOSED NORTH CENTRAL TRUNK SEWER
- EXISTING NORTH CENTRAL TRUNK SEWER



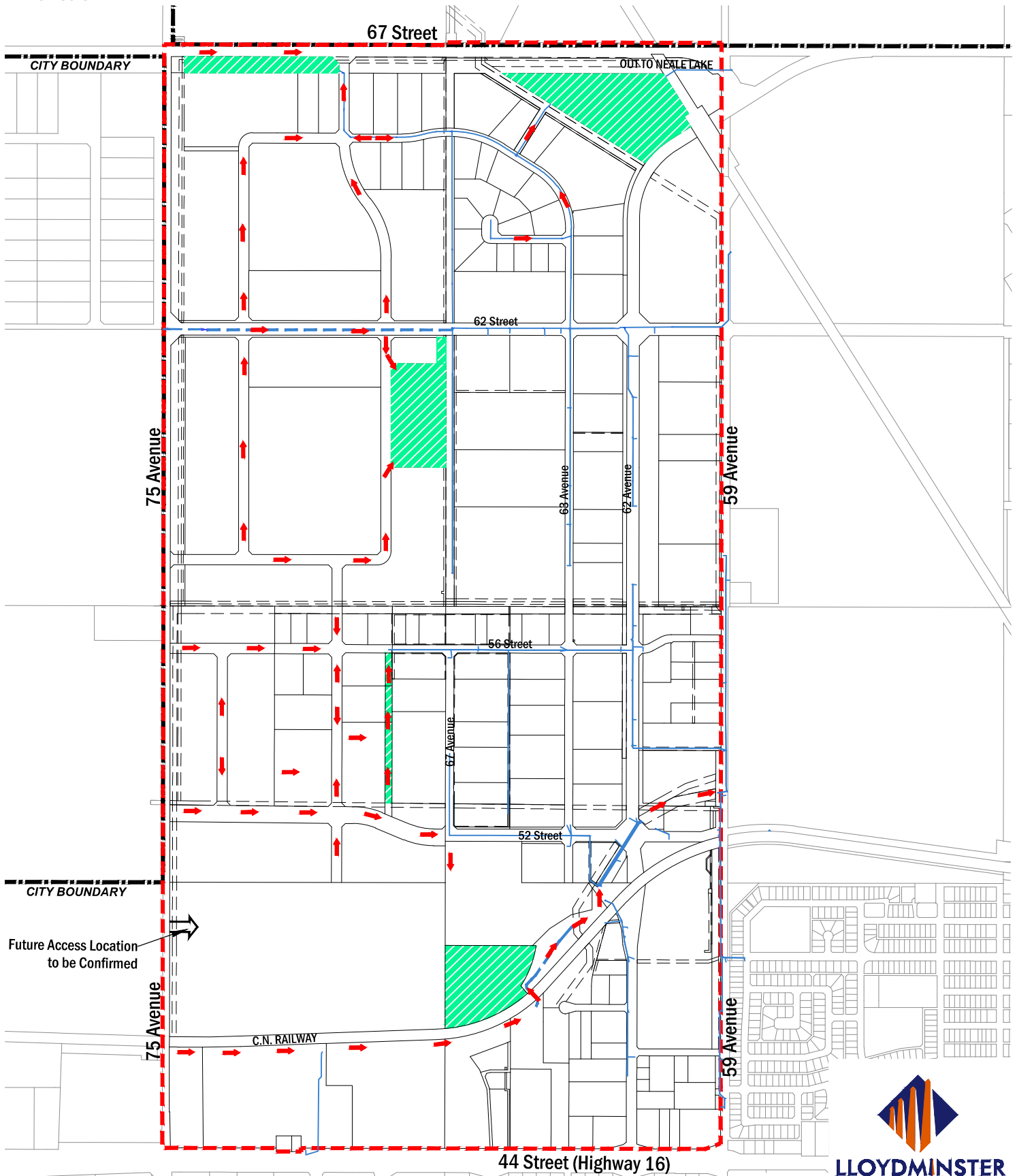
**SANITARY
SERVICING CONCEPT**



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FIGURE 4



Future Access Location to be Confirmed

STORMWATER SERVICING CONCEPT

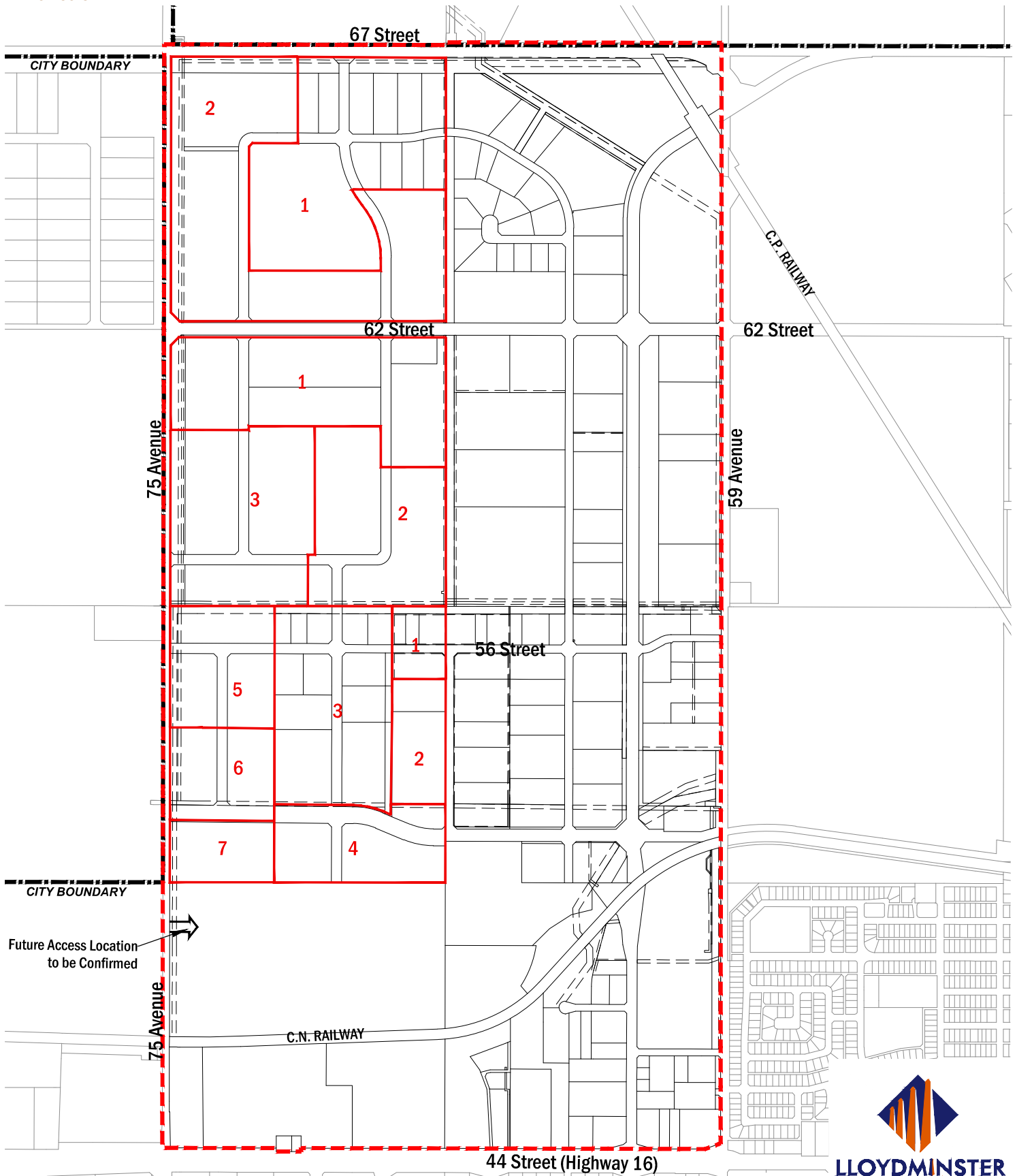
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- LEGEND**
- ASP BOUNDARY
 - PROPOSED STORM SERVICE
 - EXISTING STORM SERVICE

- OVERLAND DRAINAGE
- STORMWATER MANAGEMENT FACILITY

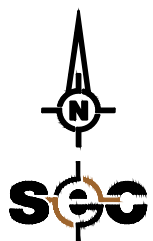
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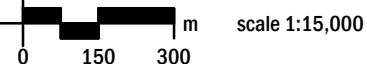


LEGEND

- - - - - ASP BOUNDARY
- STAGING BOUNDARY



STAGING CONCEPT



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